

CITY OF KIRKLAND

123 FIFTH AVENUE, KIRKLAND WASHINGTON 98033-6189 (425) 587-3800

DEPARTMENT OF PUBLIC WORKS PRE-APPROVED PLANS POLICY

Policy R-5: Curb Ramp Installation and Replacement Policy

This policy is intended to provide clear direction on when curb ramps meeting ADA standards must be installed. See the following matrix for more information.

Description of Work	New Construction	Alteration	Maintenance	Comments
Street Paving				
New street paving or overlay through an intersection (Figures 1 & 2)	X			Sidewalk ramps are required on all corners of the intersections where pedestrian crossings/crosswalks are permitted and curbing or sidewalk is present as impacted by paving.
Slurry seal through an intersection			X	No action required.
New street paving or overlay (Figure 3)		X		Sidewalk ramps are required on the leg of the intersections where pedestrian crossings are permitted and curbing or sidewalk is present. Half-street overlays trigger new curb ramp installation.
Private Construction	NC	A	M	
Construction of any new structure, including a new single family residence, and where installation of public improvements has not been deferred	X			Sidewalk ramps are required on all corners of the intersections and at mid-block locations where pedestrian crossings are permitted and curbing or sidewalk is present on project frontage.
Addition or remodel of an existing structure that exceeds the 50% threshold for multi-family or non-residential projects or \$200,000 for single family alterations as defined by the Kirkland Zoning Code and the Uniform Building Code as amended by the City of Kirkland		X		Sidewalk ramps are required on all corners of the intersections and at mid-block locations where pedestrian crossings are permitted and curbing or sidewalk is present on project frontage.

Signals and Crosswalks	NC	A	M	
Construct new traffic signal, including installation of accessible pedestrian signal equipment (Figure 4)	X			Sidewalk ramps are required on all corners of the intersections where pedestrian crossings/crosswalks are permitted and curbing or sidewalk is present as impacted by construction. See City of Kirkland Policy R-31 Accessible Pedestrian Signal Policy for detail on APS requirements.
Upgrade the existing traffic signal	X	X		Sidewalk ramps are required on all corners of the intersections where pedestrian crossings/crosswalks are permitted and curbing or sidewalk is present as impacted by construction.
Routine maintenance on a traffic signal where there is no alteration to the sidewalk or street.			X	No action required.
Installation of a new pedestrian crosswalk (Figure 5)	X			Installation of a new crosswalk where curbing is a barrier to the pedestrian walkway requires new curb ramps. Placement of thermoplastic/paint does not trigger an upgrade to existing curb ramps. Curb ramps will be evaluated on a case-by-case basis for raised crosswalks.
Sidewalks and Trails	NC	A	M	
New or reconstructed sidewalk or trail construction up to and/or within a corner's radius midpoint (Figure 6)	X			Sidewalk ramps, including receiving ramps, shall be upgraded/constructed on the leg of the intersection that the new sidewalk or trail approaches where curbing or sidewalk is present. The ramps on the corner's adjacent leg shall be upgraded/constructed if the proposed construction extends beyond the midpoint (>50%) of the corner's radius.
Reconstruct sidewalk midblock (Figure 6)			X	No action required unless a curb ramp is impacted by the replacement.
New or reconstructed sidewalk or trail that abuts or crosses an existing driveway or alley approach	X	X		Replace driveway/alley approach per City standards, or construct and route a 5-foot wide minimum accessible sidewalk at the back of the driveway/alley. Layout shall be as acceptable to the City. Existing single family homes are exempt from improving driveways other than their own, and for constructing alley approach improvements.

Curb and Gutter	NC	A	M	
New or reconstructed curb or curb and gutter up to or within a corner's radius midpoint (Figure 7)	X	X		Sidewalk ramps shall be upgraded/constructed on the leg of the intersection that the new curb impacts and/or touches.
Replacing curb or curb and gutter midblock			X	No action required as long as not in the crosswalk.
Utility Improvements	NC	A	M	
Utility excavations through an intersection		X		For curb ramps that are impacted by trenching or excavation for utility installation, construct new or replace existing sidewalk ramps within the intersection where pedestrian crossings are permitted and where curbing or sidewalk is present.
Utility excavations that impact more than 50% of a block's width (regardless of length) and extend up to or beyond a corner's radius		X		See STREET PAVING category, Figure 3.
Planter strip trenching, regardless of location, that does not impact a curb ramp or curb and gutter.			X	No action required.
On-street Parking	NC	A	M	
Repaint existing on-street parking stalls			X	No action required.
Layout modifications to existing on-street parking stalls (e.g. change parallel parking to angle parking)		X		Stripe new ADA accessible stalls, construct new ramps to provide sidewalk access.
On-Street parking stall striping associated with new construction (e.g. new pavement/overlay, new parking areas within planter strip/ROW, etc.)	X			Improvements to be determined on a case-by-case basis.

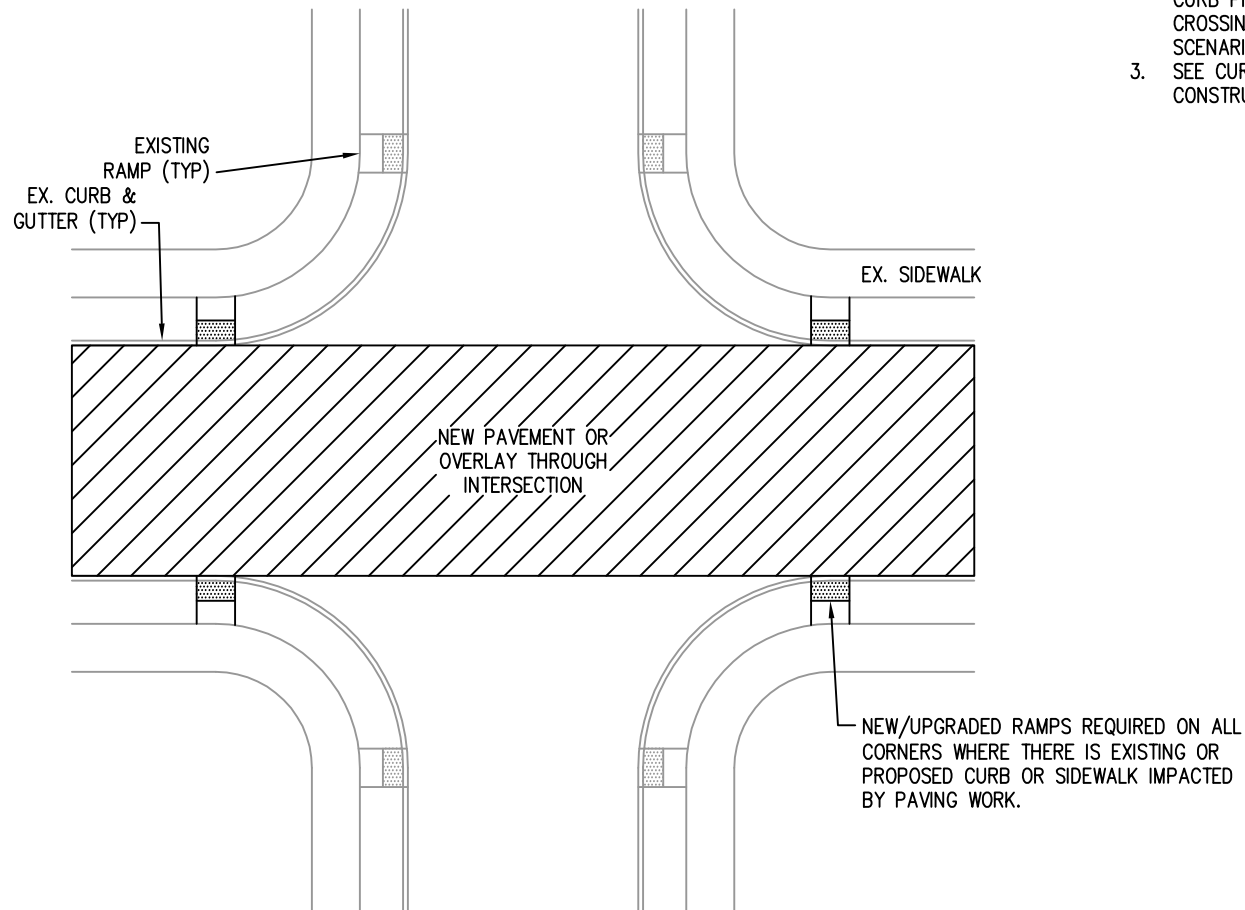
On-Street accessible parking stalls requested by citizens or commercial businesses		X		Improvements to be determined on a case-by-case basis.
Miscellaneous Striping	NC	A	M	
Installation of Bike Lanes			X	No action required.
Lane Reconfiguration			X	No action required.
Receiving Ramp Const.	NC	A	M	
If a ramp is upgraded/constructed and an existing receiving ramp is present, will the existing receiving ramp be upgraded or reconstructed?	X	X		No action required.
If a ramp is upgraded/constructed and an existing receiving ramp is NOT present, will a new ramp be constructed?	X			If the crosswalk connects to a pedestrian facility (sidewalk), then a curb ramp shall be installed.
Other	NC	A	M	
Tree installation or removal without curb, gutter, or sidewalk impacts	X	X	X	Does not change use, no action required. Maintain clear accessible path in accordance with City requirements.
Speed hump installation without impact to curb, gutter, sidewalk, or pedestrian route.	X	X		Does not change use, no action required.
Traffic Circle	X			Traffic circles will be evaluated on a case-by-case basis.
Roundabout	X			See street paving for standards.
Pedestrian Island	X			Construct new, replace existing or retrofit sidewalk ramps (including receiving ramps) for the crossing that includes the pedestrian island.

General Notes

- All curb ramp construction/upgrades shall be built concurrent with all other planned work.
- Arterial and mid-block crossings shall be as approved by the City.
- All roadway restoration shall be in accordance with City Pre-Approved Plans and Policies.
- Directional ramps are preferred over 45-degree ramps.
- Forty-Five (45) degree ramps shall be installed only after approval by the City's ADA coordinator and/or the Construction Division Manager.
- All curb ramp designs shall be stamped by a licensed Professional Engineer. If meeting the current design standards is not possible, ramps shall be constructed to the maximum extent feasible as indicated by an Engineer's note on the stamped drawings. Rationale supporting the design variance shall include a description of the scope of work, the site-specific factors affecting compliance, and the measures implemented to improve conditions. A full Maximum Extent Feasible (MEF) form shall be provided to the City by the Project Engineer with the as-builts after construction.
- All curb ramp installation and upgrades must be included in the area of impact for environmental, stormwater, and construction stormwater design and permitting.

NOTES:

1. SEE FIGURES 2 AND 3 FOR ADDITIONAL STREET PAVING REQUIREMENTS.
2. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK OR CURB PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
3. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.



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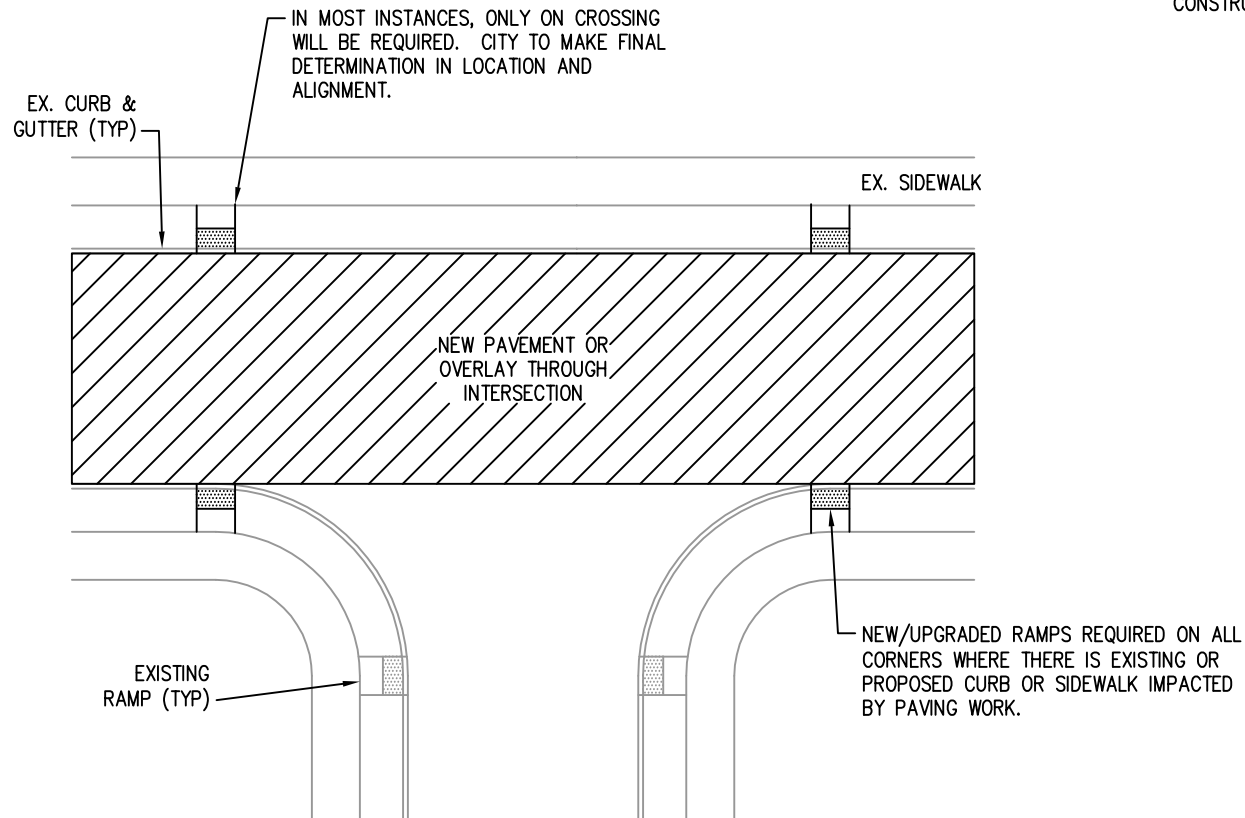
POLICY R-5, FIGURE 1



STREET PAVING
IMPROVEMENTS
FULL INTERSECTION

NOTES:

1. SEE FIGURES 1 AND 3 FOR ADDITIONAL STREET PAVING REQUIREMENTS.
2. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK OR CURB PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
3. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.



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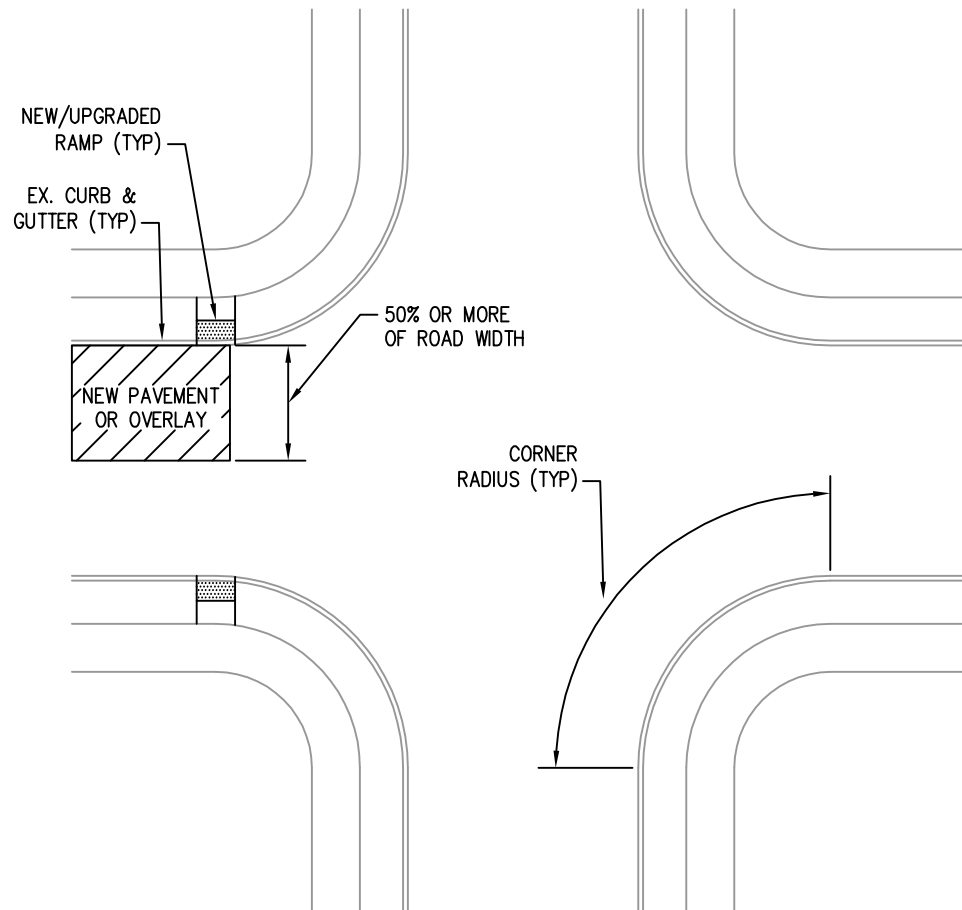
POLICY R-5, FIGURE 2



STREET PAVING
IMPROVEMENTS
T-INTERSECTION

NOTES:

1. NEW/UPGRADED RAMPS REQUIRED WHEN:
 - PAVEMENT/OVERLAY EXTENDS UP TO OR WITHIN THE CORNER'S RADIUS
 - PAVEMENT OVERLAY TOUCHES A CURB RAMP.
2. FOR RAMP REQUIREMENTS WHEN THE PAVING/OVERLAY EXTENDS INTO THE INTERSECTION, SEE FIGURES 1 AND 2.
3. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK OR CURB PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
4. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.



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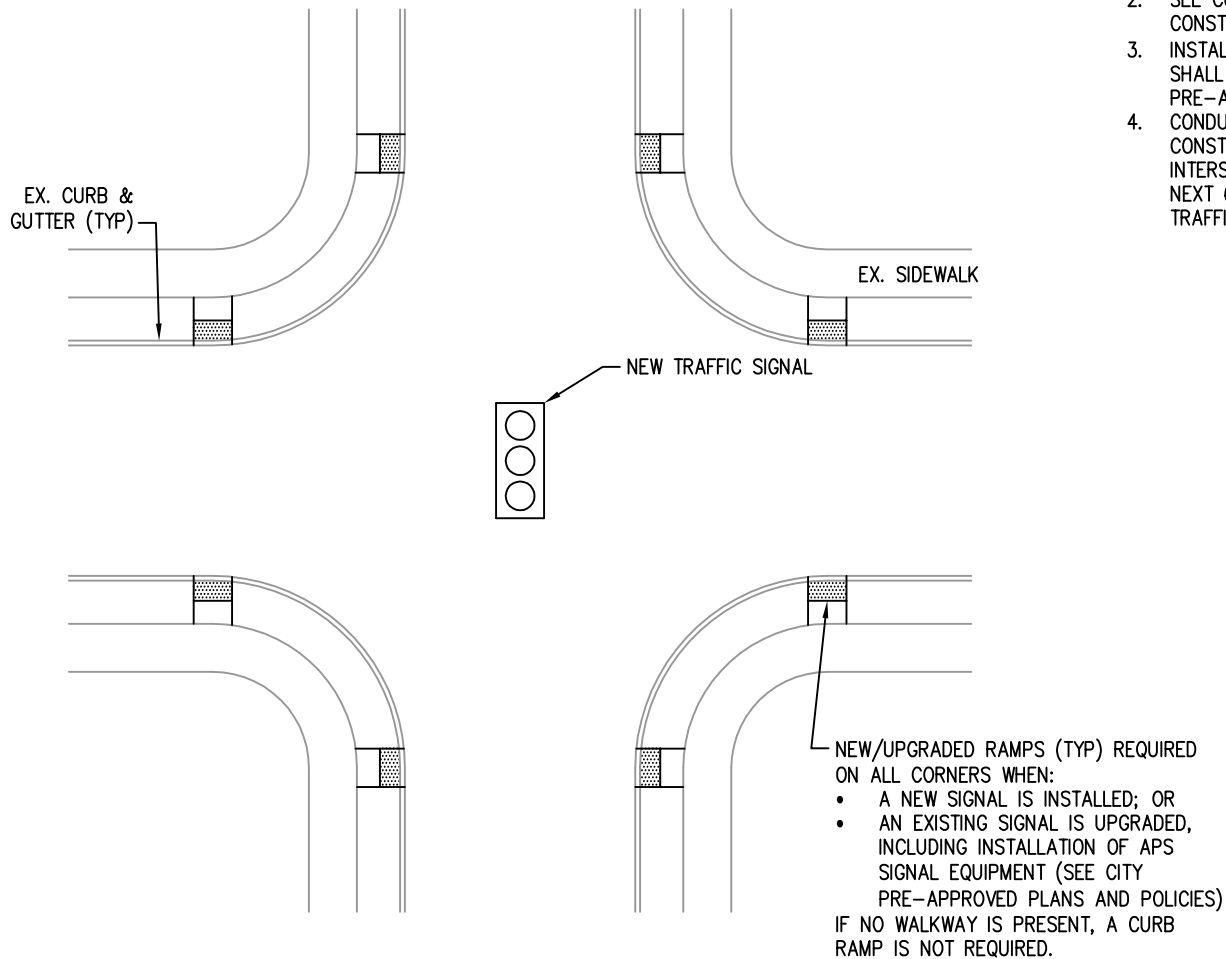
POLICY R-5, FIGURE 3



STREET PAVING
IMPROVEMENTS
ABUTTING INTERSECTION

NOTES:

1. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
2. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.
3. INSTALLATION OF ACCESSIBLE PEDESTRIAN SIGNAL (APS) SYSTEMS SHALL COMPLY WITH CITY OF KIRKLAND POLICIES AND PRE-APPROVED PLANS.
4. CONDUIT FOR APS SHALL BE INSTALLED DURING CURB RAMP CONSTRUCTION AT ALL SIGNALIZED INTERSECTIONS AND AT INTERSECTIONS WHERE SIGNALIZATION IS ANTICIPATED WITHIN THE NEXT 6 YEARS. COORDINATE WITH THE PUBLIC WORKS ENGINEERING TRAFFIC SECTION.



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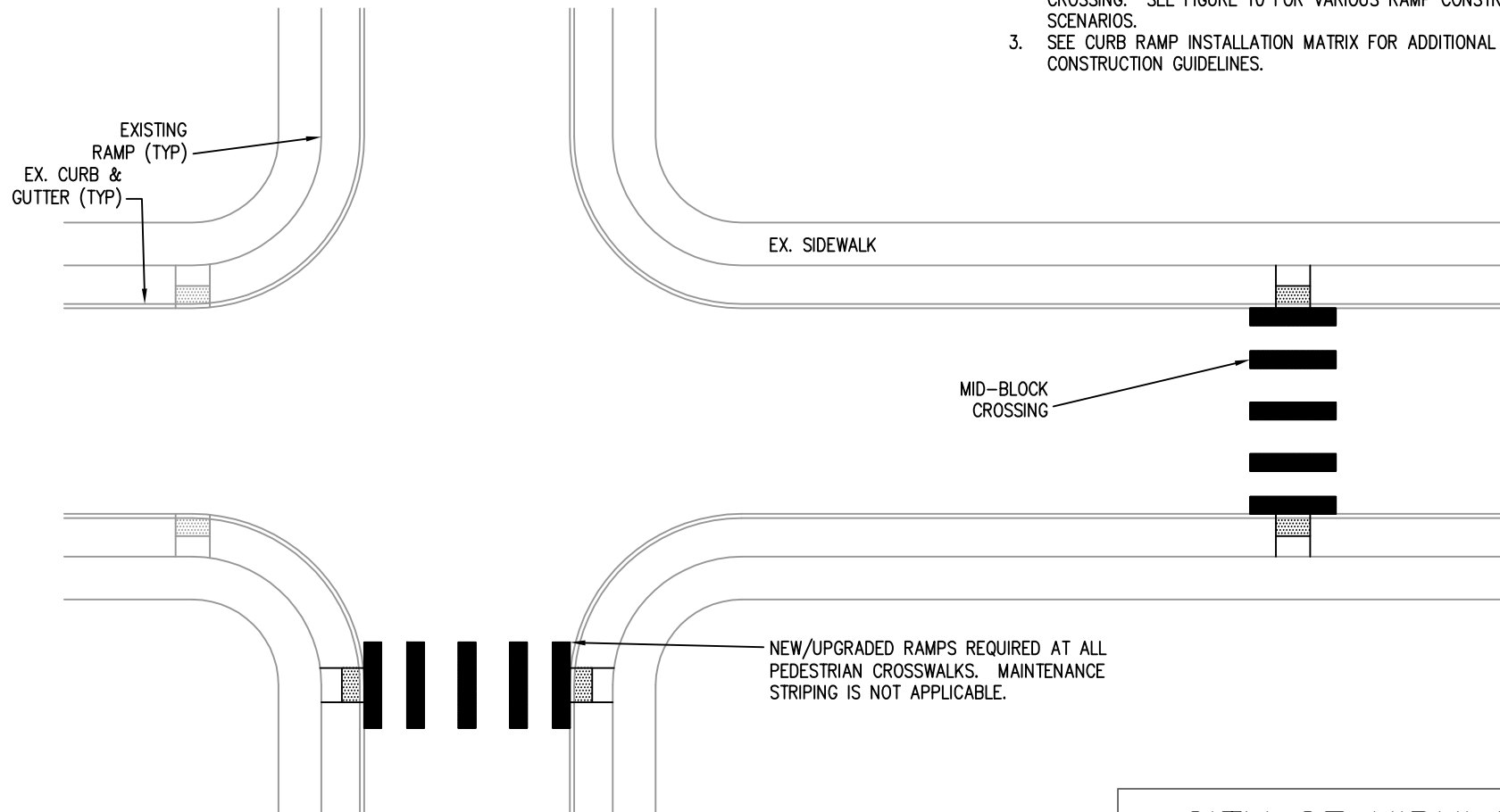
POLICY R-5, FIGURE 4



TRAFFIC SIGNAL IMPROVEMENTS

NOTES:

1. THERMOPLASTIC SHALL BE USED FOR ALL PEDESTRIAN STRIPING.
2. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK OR CURB PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
3. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.



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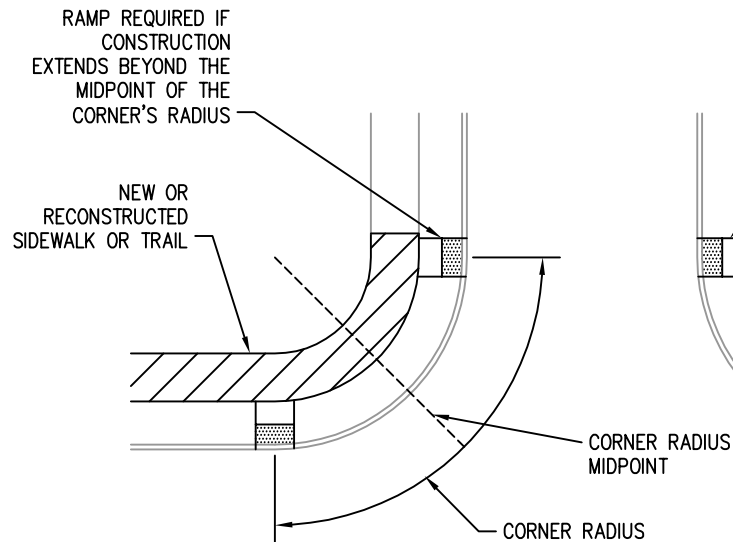
POLICY R-5, FIGURE 5



PEDESTRIAN
CROSSWALK
IMPROVEMENTS

NOTES:

1. NEW/UPGRADED RAMPS REQUIRED WHEN NEW OR RECONSTRUCTED SIDEWALK OR TRAIL IS INSTALLED UP TO OR WITHIN THE CORNER'S RADIUS.
2. RAMP UPGRADE/CONSTRUCTION IS REQUIRED ON THE LEG OF THE INTERSECTION THE CONSTRUCTION APPROACHES, INCLUDING ON THE ADJACENT LEG IF CONSTRUCTION EXTENDS BEYOND THE MIDPOINT OF THE CORNER'S RADIUS.
3. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
4. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.



RECEIVING RAMP NOT REQUIRED TO BE COMPLIANT. INSTALL NEW IF NONE EXISTS.

EX. SIDEWALK

RECEIVING RAMP NOT REQUIRED TO BE COMPLIANT. INSTALL NEW IF NONE EXISTS.

EX. CURB & GUTTER (TYP)

RECONSTRUCT SIDEWALK MID-BLOCK/OUTSIDE THE CORNER RADIUS, NO RAMPS REQUIRED.

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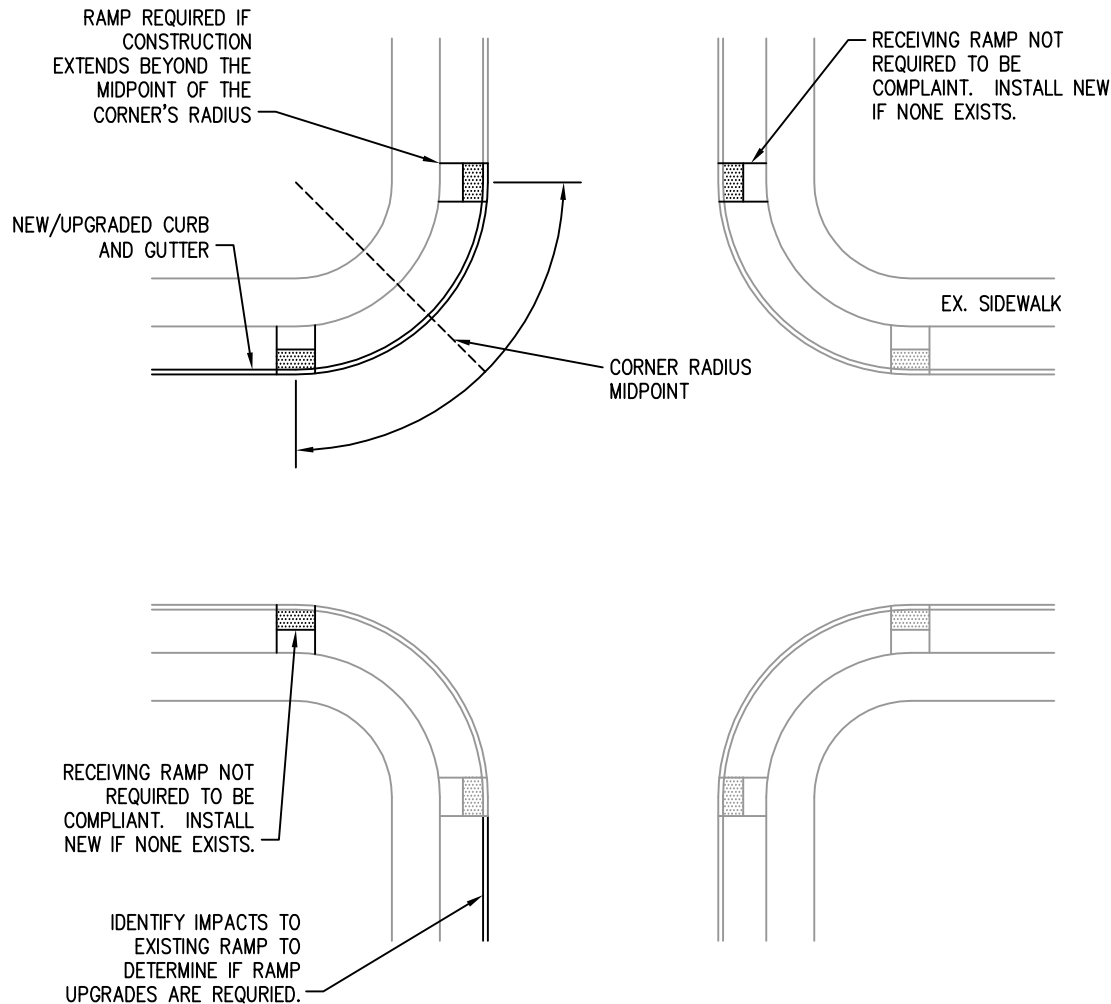
POLICY R-5, FIGURE 6



SIDEWALK AND TRAIL IMPROVEMENTS

NOTES:

1. NEW/UPGRADED RAMPS REQUIRED WHEN NEW/UPGRADED CURB AND/OR GUTTER IMPACTS OR TOUCHES THE CURB RAMP.
2. RAMP UPGRADE/CONSTRUCTION IS REQUIRED ON THE LEG OF THE INTERSECTION THE CONSTRUCTION APPROACHES, INCLUDING ON THE ADJACENT LEG IF CONSTRUCTION EXTENDS BEYOND THE MIDPOINT OF THE CORNER'S RADIUS.
3. IF RAMPS ARE LOCATED OUTSIDE THE CORNER'S RADIUS, BUT IMPACTED BY CURB AND/OR GUTTER IMPROVEMENTS, RAMPS SHALL BE REPLACED OR UPGRADED AS DESCRIBED ABOVE OR AS REQUIRED BY THE CITY.
4. RAMP CONSTRUCTION IS REQUIRED IF THERE IS A SIDEWALK OR CURB PRESENT AT THE AFFECTED END OF THE PEDESTRIAN CROSSING. SEE FIGURE 10 FOR VARIOUS RAMP CONSTRUCTION SCENARIOS.
5. SEE CURB RAMP INSTALLATION MATRIX FOR ADDITIONAL RAMP CONSTRUCTION GUIDELINES.



CITY OF KIRKLAND

POLICY R-5, FIGURE 7



CURB AND GUTTER
IMPROVEMENTS